



**STAFF REPORT TO THE BOARD OF ARCHITECTURAL REVIEW
DEPARTMENT OF COMMUNITY DEVELOPMENT
Prepared November 30, 2021**

HEARING DATE: December 9, 2021

STAFF CONTACT: Jaimie Reavis, Senior Planner

NOTIFICATION:

- Notice of Application posted on site and mailed to surrounding properties and agencies with jurisdiction on July 16, 2020.
- Notice of Public Hearing published in the Seattle Times, posted on site, and mailed to surrounding properties and agencies with jurisdiction November 24, 2021.

FILE NUMBERS: L19-0077 (Public Hearing Design Review)

ASSOCIATED FILES: L19-0078 (Lot Consolidation)
PL19-0060 (Parent File)
L03-017/E03-003 (Design Review and SEPA for Restaurant Building)

APPLICANT: David Thorstad, for Mario Galliano

REQUEST: Public hearing design review for a new three-story mixed-use building. The new building will be located to the north of the existing restaurant, which is proposed to remain. The new building is proposed to include one commercial tenant space, indoor recreation space, and a parking garage with 13 parking spaces and trash/recycling area on the first story. A total of nine dwelling units are proposed on the second and third stories.

LOCATION: 14181 Interurban Ave S (new building address).
14201 Interurban Ave S (existing restaurant building address). Project parcels include 3365901515, 3365901505, 3365901480, and 3365901470.

COMPREHENSIVE PLAN/
ZONING DESIGNATION: Regional Commercial Mixed-Use (RCM)

SEPA COMPLIANCE: New construction of up to nine residential dwelling units is exempt from SEPA review under flexible thresholds the City of Tukwila has adopted under TMC 21.04.110, consistent with allowable flexible thresholds in WAC 197-11-800 (1)(c).

PUBLIC COMMENTS: Two comment letters were received from neighboring residential property owners in response to the Notice of Application for this project, they are included as Attachment B to this report.

RECOMMENDATIONS: **L19-0077 Public Hearing Design Review**
Staff recommends approval with conditions of the Public Hearing Design Review application.

ATTACHMENTS:

- A. Applicant's response to design criteria
- B. Comment Letters and Responses
- C. Geotechnical Report, dated October 2018 and Review Letter from Geotechnical Engineer titled Geotechnical Review of Final Plans, dated August 10, 2021
- D. Review Letter from Waste Management
- E. Plan Sheets
 - Site Plans: Site Plan (1), Site Plan (1) - with Fire Department Redlines
 - Building Floor Plans: First Floor Plan (5.5), Second Floor Plan (7.5), Third Floor Plan (9.5)
 - Building Elevations (12, 13)
 - Landscaping Plans (L1.1, L1.2, L3.1, L3.2, L3.3)
 - Site Survey (Sheet 1 of 2, Sheet 2 of 2)
 - Site Lighting Plan

FINDINGS

PROJECT BACKGROUND

The applicant is proposing to construct a three-story mixed-use building at the southwest corner of Interurban Ave S and 58th Ave S. Proposed uses within the building include one commercial tenant space and nine apartments. The first floor of the building will consist of the commercial tenant space and areas associated with the apartments, including the apartment building entrance and lobby, an indoor recreation space/gym for apartment residents, and a parking garage with 13 parking spaces and the trash/recycling area. Additional parking spaces for the uses within the building are proposed outside of the building, in the rear area of the property (south of the new building and behind the restaurant). The second floor of the building will have five dwelling units, including four 2-bedroom units and one 1-bedroom unit. The third floor will have four dwelling units, including two 2-bedroom units and two 3-bedroom units. Each dwelling unit includes a private balcony area. The building's floor area is approximately 20,407 square feet and will have a footprint of approximately 7,400 square feet.

ZONING

The project is located within the Regional Commercial Mixed-Use (RCM) District which allows up to 14.5 dwelling units per net acre. The purpose of the RCM district per TMC 18.26.010 is as follows:

It is intended to provide for areas characterized by commercial services, offices, lodging, entertainment, and retail activities with associated warehousing, and accessory light industrial uses, along a transportation corridor and intended for high-intensity regional uses. Residential uses mixed with certain commercial uses are allowed at second story or above. The zone's standards are intended to promote attractive development, an open and pleasant street appearance, and compatibility with adjacent residential areas.

Almost all the properties with frontage along Interurban Ave S are zoned either RCM or Commercial/Light Industrial (C/LI). Residential areas of Tukwila are generally located west of the project site, although there are a few areas of the frontage of Interurban Ave S zoned for residential development, including the Foster Greens apartments located approximately ¼ mile to the north. Other nearby residential zones and development along the corridor tend to be located on the hillside above the street-level of Interurban Ave S.

EXISTING CONDITIONS

Existing Development

The site is currently made up of four parcels that were developed in 2003-2004 with a 3,000 square foot restaurant and deli and 48 parking stalls. The applicant has submitted a lot consolidation application to combine all four lots into one property, which is proposed to include the restaurant, new mixed-use building, and associated parking and landscaping for both. Figure 1 shows the development site including all four parcels; Figure 2 shows the specific area of the site where the new building and associated improvements are proposed.



Figure 1: Overall Project Site (including restaurant building and parking area to the south)



Figure 2: Building Site



Figure 3: Project Site Frontage along Interurban Ave S



Figure 4: Project Site Frontage along 58th Ave S

Surrounding Land Uses:

Offices and casinos are located across the street adjacent to Interurban Ave. S. There are a handful of single-family residential structures to the south, the nearest of which have been converted to an office and a church. To the north, on the other side of 58th Ave S is located a one-story shop housing a plumbing business. Single-family residential properties are directly adjacent to the property's west side where the homes are located at least 65 feet up above the elevation of the project site, at the top of the slope on S 142nd St. and 59th Ave S. The yards of

these residential properties include the heavily vegetated hillside which slopes down to the project site.



Figure 5: Surrounding Land Uses and Development

Topography: The portion of the site that will be developed is flat except for a drainage swale along the western edge proposed to be filled in with structural fill. Immediately west of the drainage swale is a steep hillside, which is an environmentally critical area containing Class 3 landslide hazard areas (see Figure 6). The hillside is heavily vegetated and on residential property belonging to the adjacent single-family residents.

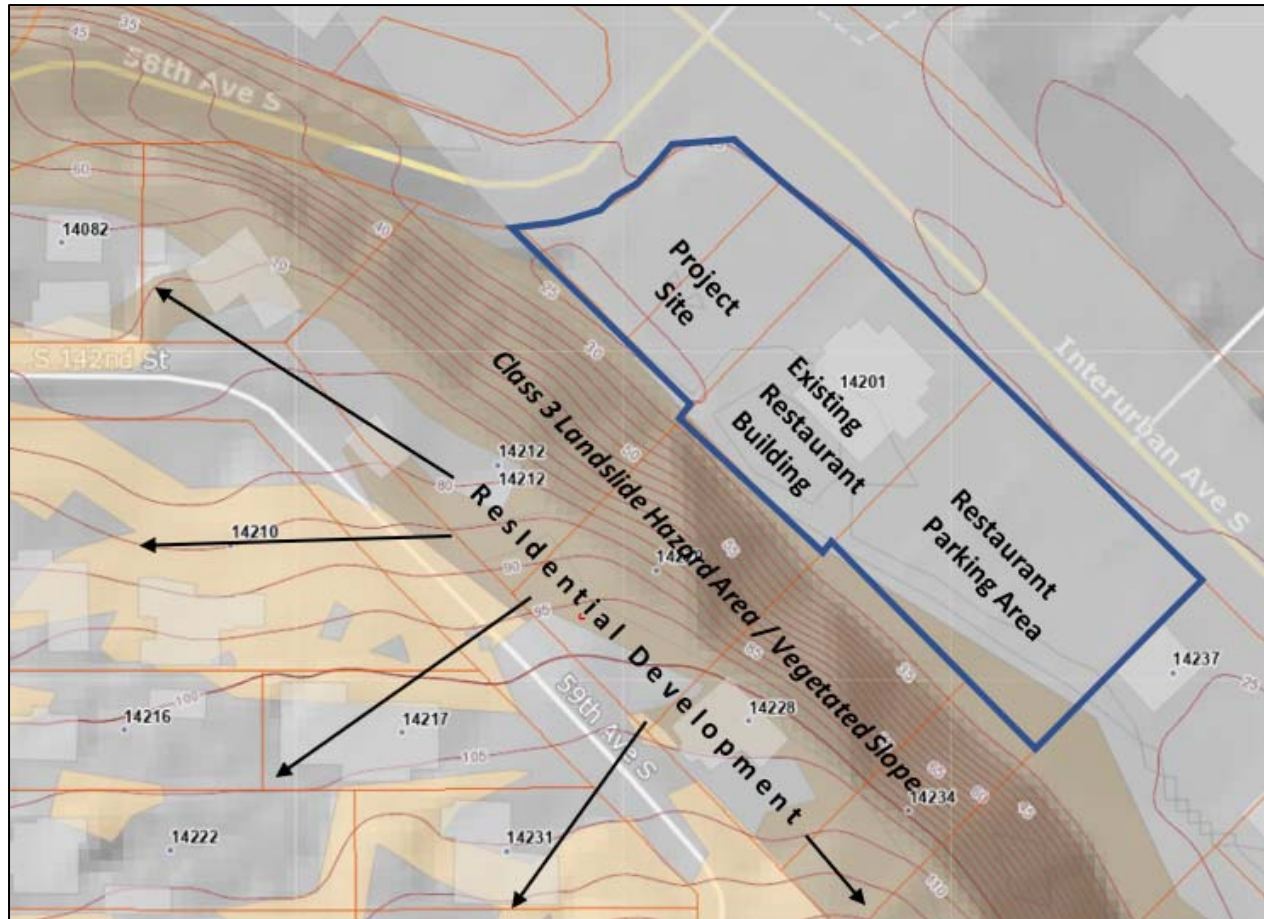


Figure 6: Site, Topography, and Adjacent Development

Vegetation:

Existing vegetation on the site includes trees, shrubs, and groundcover planted as part of development of the restaurant. A total of six trees planted as part of the original restaurant development landscaping are proposed to be removed and replaced with new trees and other required landscaping as part of the new mixed-use building development. All vegetation on the hillside is on adjacent residential properties and is required to be protected by City code.

DEVELOPMENT STANDARDS – REGIONAL COMMERCIAL MIXED-USE (RCM)

The following development standards apply to projects within the RCM zoning district; an analysis of project compliance is included below:

- Setbacks, minimum:
 - Front (Interurban Ave S) = 20' – **Complies**
 - Second Front (58th Ave S) = 10' – **Complies**
 - Side (adjacent to restaurant building) = 10'
 - Rear (adjacent to hillside/LDR zone) = tiered as follows:
 - 1st floor = 10' - **Complies**
 - 2nd floor = 20'- **Complies**

- 3rd floor = 30' – **Complies**
- Height, maximum: 3 stories or 35' – Building is 30 feet tall except for elevator shaft which comes up from the roof to a height of 34'6." - **Complies.**
- Landscape
 - Fronts (Interurban Ave S and 58th Ave S) = 10', Type 1 – **Complies**
 - Side and Rear: 10', Type 3 – **Complies**
- Parking Lot Landscaping – **Not applicable** – There are no new surface parking areas proposed with this project.
- Recreation space: 200 square feet per dwelling unit – **Complies (see Table 1 below)**
- Parking – **Complies (see Table 2 below)**

Table 1: Recreation Space Calculations

Recreation Space Square Footage	Shared Space	Front Units	Middle Unit (rear)	Rear Units	TOTAL
First Floor	697 (indoor gym)				697
Second Floor		96 (x2) = 192	280	290 (x2) = 580	1,052
Third Floor		96 (x2) = 192		516 (x2) = 1,032	1,224
TOTAL	697 square feet (SHARED)	2,276 square feet (PRIVATE)			2973 (average of 330 sf/unit)

Table 2: Parking Calculations

Land Use	Parking Spaces Required by Zoning Code	Number of Dwelling Units/Usable Floor Area (UFA)	Total Parking Spaces Required
Dwelling Units	2/unit	9 units (each is 3 bedrooms or less)	18
Commercial Tenant Space	Office use = 3/1,000 UFA Retail use = 4/1,000 UFA	622 square feet	2
Existing Restaurant	1/100 UFA	2,600 square feet	26
TOTAL SPACES PROPOSED = 46			46

DESIGN REVIEW CRITERIA

The project is subject to Public Hearing Design Review as required by TMC 18.26.070. The Multi-Family, Hotel and Motel Design Review Criteria contained in TMC 18.60.050 (C) are the criteria applicable to this project. The review criteria are divided into four sections, including (1) Site Planning, (2) Building Design, (3) Landscape and Site Treatment, and (4) Miscellaneous Structures. The following is a discussion of the proposal in relation to the criteria. Design review criteria are shown in italics, followed by staff's analysis in regular text. For the applicant's response to the criteria, please see Attachment A.

- **SITE PLANNING**

- a. *Building siting, architecture, and landscaping shall be integrated into and blend harmoniously with the neighborhood building scale, natural environment, and development characteristics as envisioned in the Comprehensive Plan. For instance, a multi-family development's design need not be harmoniously integrated with adjacent single-family structures if that existing single-family use is designated as "Commercial" or "High-Density Residential" in the Comprehensive Plan. However, a "Low-Density Residential" (detached single-family) designation would require such harmonious design integration.*

- The proposed height and scale of the project, at three stories, is in keeping with office and residential development along Interurban Ave S. The project has a tiered setback on the rear side of the building where it is adjacent to residential properties.
- Located on a visually prominent corner property, the proposed mixed-use building is adjacent to the 58th Ave S gateway/entry point to the Tukwila Hill neighborhood and will provide a blend of commercial and residential uses where the commercial development typical along Interurban Ave S transitions to the residential neighborhood above.



Figure 7: Corner Site/Gateway to Residential Neighborhood

- b. *Natural features, which contribute to desirable neighborhood character, shall be preserved to the maximum extent possible. Natural features include, but are not*

limited to, existing significant trees and stands of trees, wetlands, streams, and significant topographic features.

- An area of dense vegetation including many significant trees is located on the hillside owned by the residential property owners to the west. This area is not proposed to be disturbed with the current project. The hillside and associated vegetation will continue to provide screening and separation of development along Interurban Ave S from residential properties to the west.



Figure 8: Vegetated Hillside at Rear of Site

- c. The site plan shall use landscaping and building shapes to form an aesthetically pleasing and pedestrian scale streetscape. This shall include, but not be limited to facilitating pedestrian travel along the street, using architecture and landscaping to provide a desirable transition from streetscape to the building, and providing an integrated linkage from pedestrian and vehicular facilities to building entries.*
 - The proposed mixed-use building is sited to face the main street of Interurban Ave S, with paved walkways linking building entrances to the public sidewalks along Interurban Ave. S. and 58th Pl. S.

d. *Pedestrian and vehicular entries shall provide a high-quality visual focus using building siting, shapes, and landscaping. Such a feature establishes a physical transition between the project and public areas and establishes the initial sense of high-quality development.*

- The landscaping plan shows pedestrian walkways connecting from the sidewalk along Interurban Ave S to each of the main building entries. The pedestrian access connecting to the commercial tenant space is aligned symmetrically with the center of the building façade and a concrete seating wall planter located in between the entry doors of the commercial tenant space and the gym/indoor recreation space for apartment residents.
- Location of vehicular entries are separated from public and pedestrian-oriented areas at the front of the building. Vehicular entries include a driveway off 58th Ave. S. connecting to a garage door on the north side of the building, and another driveway connecting to a garage door and surface parking areas on the south side of the building.

e. *Vehicular circulation design shall minimize driveway intersections with the street.*

- The site design makes use of the existing driveways used by the restaurant. The project proposes to have the existing access off Interurban Ave S on the north side of the restaurant be a shared access and service drive located between the new building and the restaurant. The driveway off 58th Ave S



Figure 9: Existing Driveways

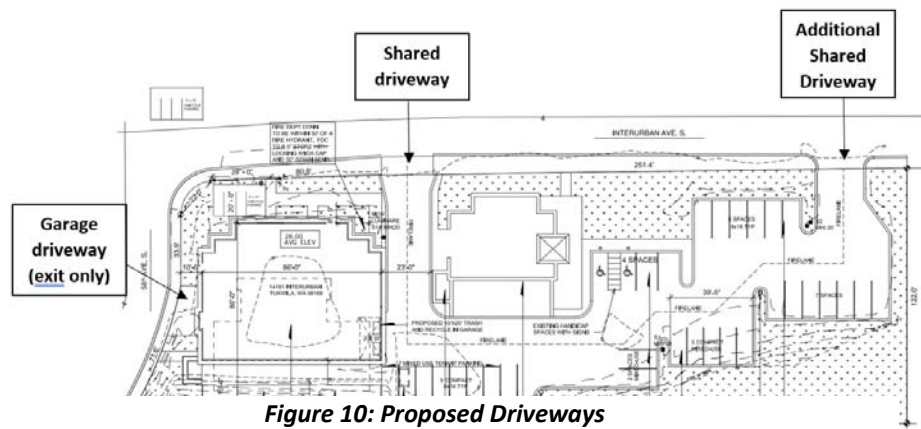


Figure 10: Proposed Driveways

will be controlled access, with use limited to those who have garage openers (i.e., apartment residents and commercial tenants).

f. *Site perimeter design (i.e., landscaping, structures, and horizontal width) shall be coordinated with site development to ensure a harmonious transition between adjacent projects.*

- Landscaping, pedestrian and vehicular access, parking, utility design, and lighting has been reviewed with respect to the existing restaurant building and the public frontage.
- City staff has been coordinating with Mr. Galliano and Seattle City Light to ensure the diagonal overhead power is undergrounded to the new building as required.

g. *Varying degrees of privacy for the individual residents shall be provided, increasing from the public right-of-way to common areas, to individual residences. This can be accomplished through the use of symbolic and actual physical barriers to define the degrees of privacy appropriate to specific site area functions.*

- The site design provides for varying degrees of privacy for the individual residents. The graphics below include a figure from the Multifamily Design Guidelines that illustrates how a site should be designed to provide physical cues indicating a transition from public to private space. The public, semi-public, semi-private, and private areas have been highlighted in the site plan in the graphic at right and are described below:

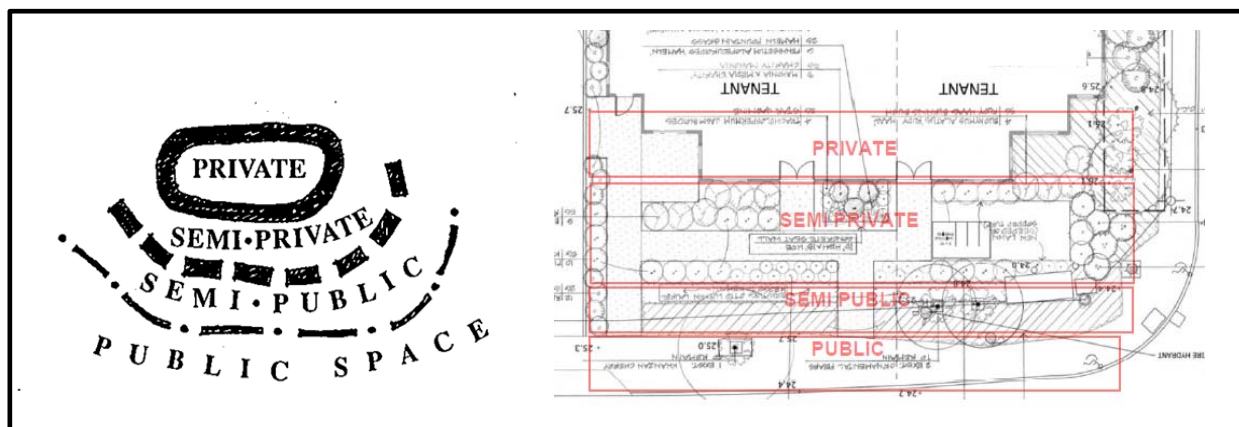


Figure 11: Site Design Transition from Public to Private Space

- Public: Sidewalks along Interurban and 58th Place S.
- Semi-Public: Landscaping at the back of sidewalk will consist of groundcover and shrubs in the area immediately adjacent to the sidewalk along Interurban, creating an open, semi-public transition to the site.
- Semi-Private: The existing hedge will remain on site and provides a symbolic barrier between the semi-public and semi-private space.

The semi-private space includes the sidewalk and lawn areas, bike rack, seating wall, and exterior entryway areas to the apartments and tenant spaces along the front of the building.

- Private: The walkway leading to the customer entrances of the tenant spaces in the building is aligned with the center of the building, creating a more visible and public entrance for visitors to those spaces. The apartment entrance is on the left side of the building façade, away from the public entrance to the commercial tenant space. While being visible from the public street, the residential entrance is designed to be more private by not being in the center of the building.
- h. Parking and service areas shall be located, designed, and screened to interrupt and reduce the visual impact of large, paved areas.*
- Parking and service areas are largely screened from view due to their location either within the parking garage on the first floor of the proposed building, or at the rear side of the property where their prominence is minimized. A total of 13 of the 20 required parking spaces are located within the parking garage, which also contains designated space for the trash and recycling bins. The remainder of the parking spaces for the new building will be surface parking spaces located on the south side of the building, in the area at the end of the access drive off Interurban Ave S shared by the restaurant and the mixed-use building.
- i. The height, bulk, footprint, and scale of each building shall be in harmony with its site and adjacent long-term structures.*
- The building footprint is generally square at 86 feet by 86 feet, with modulation along the front and rear of the building. The proposed height of the building is generally 30 feet. The elevator shaft extends up from the roof to a height of 34 feet, 8 inches. On the rear side of the building adjacent to the hillside, the building has been designed with a tiered setback as required due to its location adjacent to the Low-Density Residential zoning district. Per requirements of the RCM zone, if any portion of a side or rear yard is within 50 feet of a residential district, the setback of upper floors is required to be set back an increasing amount from adjacent residential properties. The first floor is set back 10 feet from the rear property line; the second floor 20 feet, and the third floor 30 feet. This design also allows for units on the rear side of the building to have decks that are 10 feet in depth.
 - Nearby buildings in the RCM zone include buildings with smaller and larger footprints than the building proposed and range in height from one to three stories. Buildings with smaller footprints tend to include large areas of the site devoted to surface parking. Parking inside the garage within the first floor of the proposed building allows for increased infill development on the site and provides screening of parking areas.



Figure 12: Interurban Ave S Commercial Development (north of project site)



Figure 13: Interurban Ave S Residential Development (north of project site)

- **BUILDING DESIGN**

- a. *Architectural style is not restricted; evaluation of a project shall be based on the quality of its design and its ability to harmonize building texture, shape, lines and mass with the surrounding neighborhood.*
 - The building design includes a flat roof consistent with nearby commercial development and typical of modern apartment buildings constructed in Tukwila and the region. The building design includes symmetrical, vertical modulation along the front elevation: the narrowest portion of the façade is in the center of the building, and the building becomes wider in portions set back farther away from the street front. This creates a tiered setback of the building footprint along the front property line adjacent to Interurban Ave S. Horizontal modulation is included on the rear side of the building, also creating a tiered design of the building profile along the rear yard setback.

- b. *Buildings shall be of appropriate height, scale, and design/shape to be in harmony with those existing permanent neighboring developments that are consistent with, or envisioned in, the Comprehensive Plan. This will be especially important for perimeter structures. Adjacent structures that are not in conformance with the Comprehensive Plan should be considered to be transitional. The degree of architectural harmony required should be consistent with the nonconforming structure's anticipated permanence.*
 - The building design and scale blends the predominantly commercial development typical of the RCM and C/LI zones along Interurban Ave S with the residential development located on the hill above the site, as well as the apartments along Interurban within the MDR zone to the north. The flat roof design is consistent with nearby commercial development. The inclusion of roof overhangs and corbels, along with a rich, warm color scheme that makes use of stone veneer, lap siding, and trim is consistent with nearby multifamily and single-family residential development.

- c. *Building components, such as windows, doors, eaves, parapets, stairs and decks shall be integrated into the overall building design. Particular emphasis shall be given to harmonious proportions of these components with those of adjacent developments. Building components and ancillary parts shall be consistent with the anticipated life of the structure.*
 - Proposed building components, including windows, doors, corbels, the building parapets, decks, and railings are integrated into the overall building design through use of matching or complementary colors and materials to the main structure. Each floor of the building is visually separated by two horizontal bands made up of contrasting colors - a white (Chantilly Lace) color and a deep brown (Benjamin Moore North Creek Brown). The white color of the horizontal bands helps to accentuate white-colored building details which include trim around the doors and windows, corbels, and deck railings. The deep brown trim color matches the siding color used on the

ground floor. A lighter shade of brown (Benjamin Moore Deep Ochre) is proposed on the second and third floors. Stone veneer is proposed on the most prominent portions of the first floor facing Interurban Ave S and 58th Pl S and is carried up to portions of the second story. The stone veneer ties together the two shades of brown colors proposed for the siding and the dark brown color proposed for windows and doors (including the garage doors).

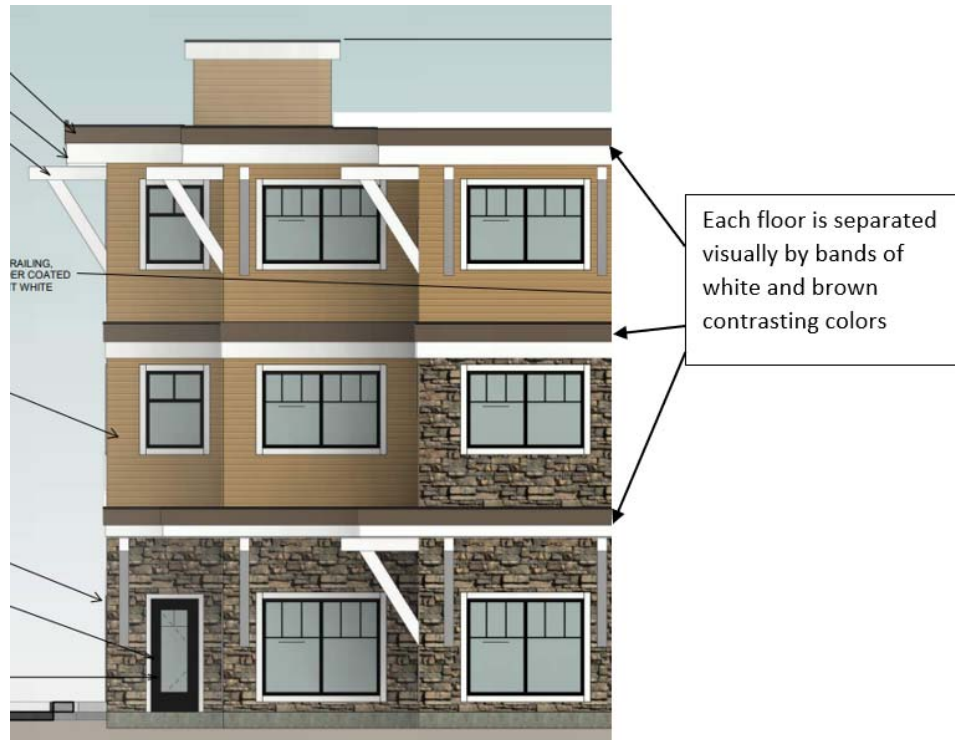


Figure 14: Building Colors and Materials

- d. *The overall color scheme shall work to reduce building prominence and shall blend in with the natural environment.*
- The earth tone color scheme proposed works to reduce the building prominence and helps the building blend in with the adjacent vegetated hillside.
- e. *Monotony of design in single or multiple building projects shall be avoided. Variety of detail, form, and siting shall be used to provide visual interest. Otherwise monotonous flat walls and uniform vertical planes of individual buildings shall be broken up with building modulation, stairs, decks, railings, and focal entries. Multiple building developments shall use siting and additional*

architectural variety to avoid inappropriate repetition of building designs and appearance to surrounding properties.

- This building is a relatively small nine-unit residential apartment building. The design includes both vertical and horizontal building modulation. The front building façade is proposed to have a seating wall planter located in between the front entry doors of the tenant space and indoor recreation space and will create a focal point aligned with the primary pedestrian connection to the building from Interurban Ave S. A variety of colors and materials are proposed on all building elevations to provide visual interest.

• **LANDSCAPE AND SITE TREATMENT**

a. *Existing natural topographic patterns and significant vegetation shall be reflected in project design when they contribute to the natural beauty of the area or are important to defining neighborhood identity or a sense of place.*

- The development site is flat, with a steep slope adjacent to the property on the rear side of the proposed building. The dense vegetation on the hillside

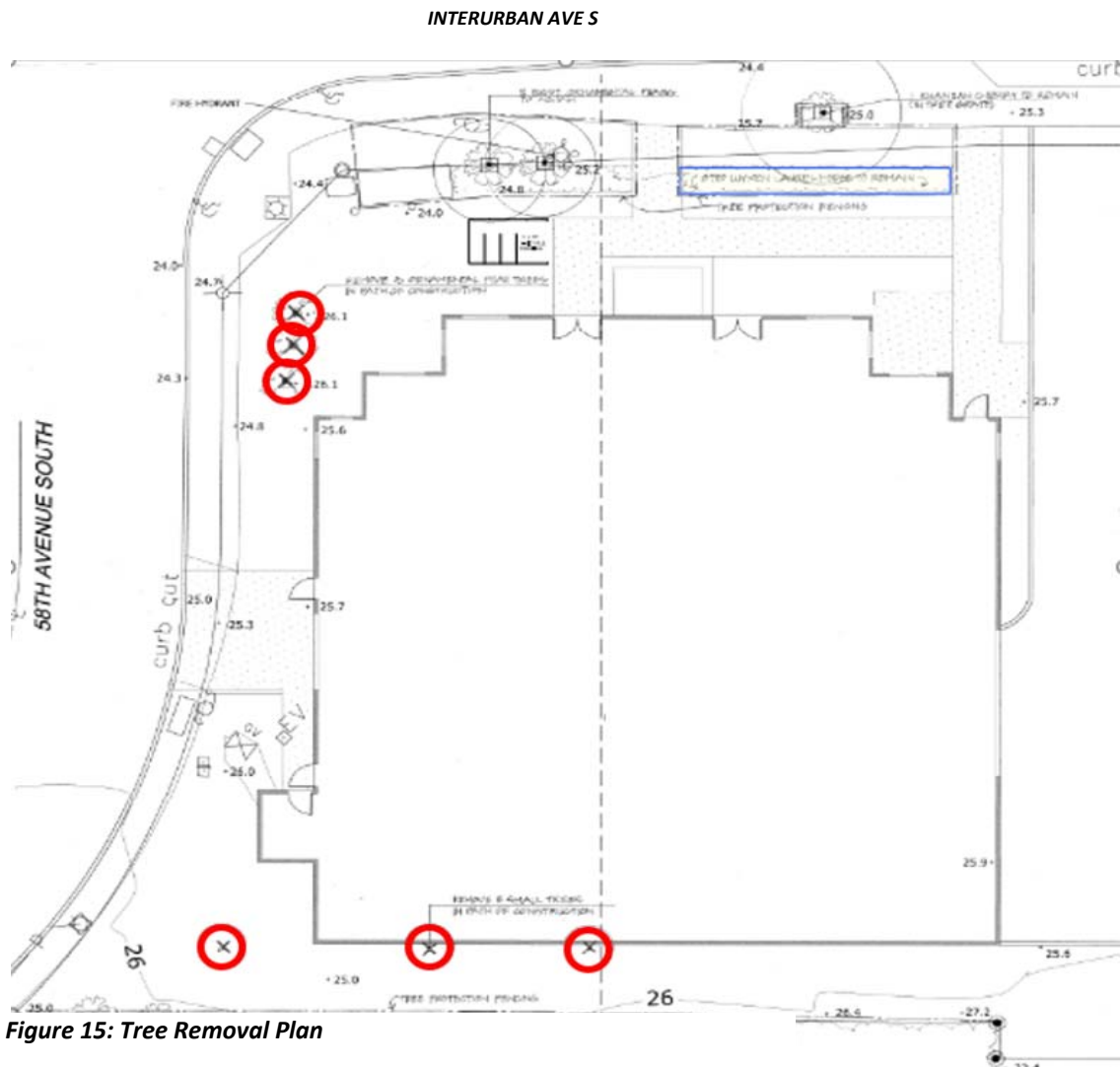


Figure 15: Tree Removal Plan

is not on the development site. Tree protection fencing is proposed to ensure there is no disturbance to this area during construction. The dense vegetation on the hillside will help provide privacy between the new multifamily units and the single-family dwellings at the top of the hillside.

- Existing street trees and mature landscaping originally planted as part of the restaurant development is proposed to be retained to the extent possible. Figure 15 shows existing trees and vegetation on site. The trees proposed to be removed are indicated with red circles around them.
- b. *Landscape treatment shall enhance existing natural and architectural features, help separate public from private spaces, strengthen vistas and important views, provide shade to moderate the effects of large, paved areas, and break up visual mass.*
- The landscape treatment helps to frame the driveway areas and walkways connecting to the building. Foundation landscaping is proposed, enhancing the front entry area. A raised-bed planter centered in between the front door of the tenant space and the indoor recreation space is consistent with the symmetrical design of the building façade and enhances the design of the pedestrian building entry.
- c. *Walkways, parking spaces, terraces, and other paved areas shall promote safety and provide an inviting and stable appearance. Direct pedestrian linkages to the public street, to on-site recreation areas, and to adjacent public recreation areas shall be provided.*
- Direct pedestrian linkages are provided from the sidewalk along Interurban Ave S to the front entrances to the tenant space, indoor recreation space, and the apartments. This is shown in Figure 16, where direct pedestrian

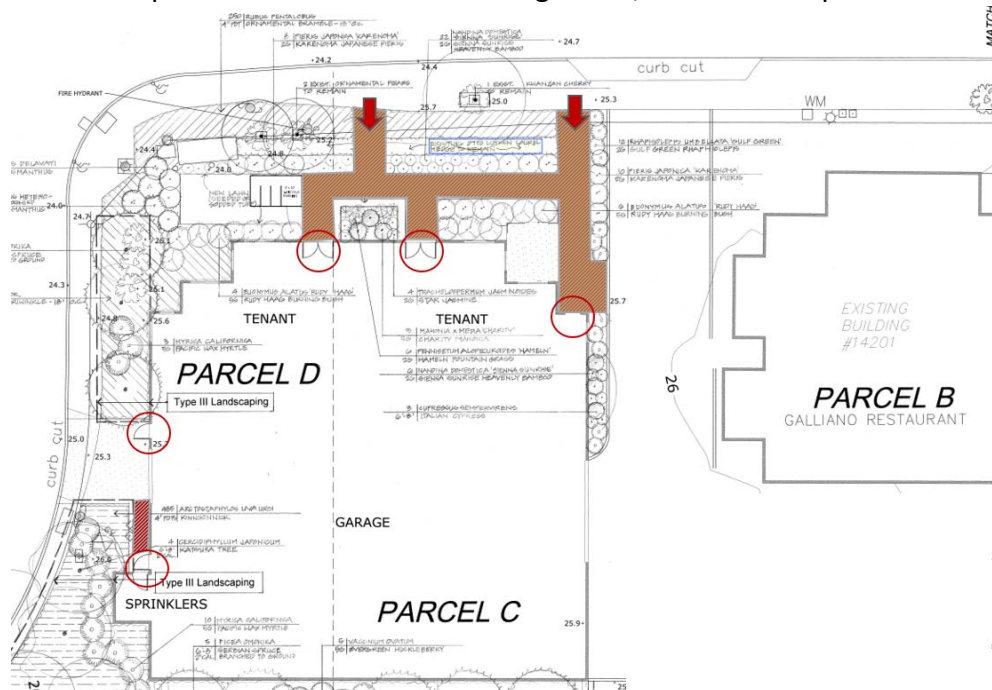


Figure 16: Pedestrian Access

linkages from the sidewalks are shown with arrows, walkways are the shaded reddish/brown areas, and pedestrian entrances to the building are circled. Walkways are framed by landscape areas, and curbs are located along the driveways and drive aisle to prevent damage from vehicles.

- d. *Appropriate landscape transition to adjoining properties shall be provided.*
- Perimeter landscaping is proposed as required along the front, second front, and rear of the property. A narrow landscape strip adjacent to the shared drive aisle is proposed. Except for the six trees to be removed, existing landscaping associated with the restaurant and the rest of the site is proposed to remain as-is.

- **MISCELLANEOUS STRUCTURES**

- a. *Miscellaneous structures shall be designed as an integral part of the architectural concept and landscape. Materials shall be compatible with buildings, scale shall be appropriate, colors shall be in harmony with buildings and surroundings, and structure proportions shall be to scale.*
- Miscellaneous structures proposed are the bike racks and the concrete seating wall planter, which are proposed to be in front of the building where they will be visible from the commercial tenant space. The bike racks will be of a standard design, in the form of a staple rack, and will be of a neutral color consistent with the design of the building. The location and space provided for the bike rack is not shown consistently among different site plans, and only one bike rack is required. Staff recommends a condition to require that the details of the bike parking design be worked out as part of the building permit, in a way which meets the required number of bike parking spaces while at the same time preserving as much green space as possible in front of the building.
 - The Fire Marshal's Office has reviewed the plans submitted for design review for compliance with emergency access requirements. Items noted in review comments and redlines to the site plan include adjustments to the location of the building address, fire department connection and associated equipment, location requirements for a new fire hydrant, and requirements to have a KNOX eLock key box for the building and a CLICK 2 ENTER for the parking garage door. Additional permits from the Tukwila Fire Department are required, and conformance with redlines will be reviewed as part of construction permit submittals.
- b. *The use of walls, fencing, planting, berms, or combinations of these shall accomplish screening of service yards and other places that tend to be unsightly. Screening shall be effective in winter and summer.*
- The trash and recycling bins are proposed to be stored inside the parking garage where they will be screened from view.
- c. *Mechanical equipment or other utility hardware on roof, ground or buildings shall be screened from view. Screening shall be designed as an integral part*

of the architecture (i.e., raised parapets and fully enclosed under roof) and landscaping.

- Mechanical plans submitted for the project show that there will be equipment visible on the building exterior, including vents, exhausts, and louvres. No rooftop mechanical equipment is shown at this time, and utility boxes and meters are also not shown. As a condition of approval, staff recommends that all mechanical equipment located on the building exterior be painted to match the color scheme of the building. If any rooftop mechanical equipment is necessary, it shall be setback from the edge of the roof and screened by the height of the parapet or otherwise screened from view. Ground equipment, including utility boxes and meters, shall be screened by project landscaping. Additional landscaping from what is shown on the plans may need to be added during project construction to screen this equipment since it has not yet been shown on the plans and is often added to a site after the building permit has already been issued.
- d. Exterior lighting standards and fixtures shall be of a design and size consistent with safety, building architecture and adjacent area. Lighting shall be shielded and restrained in design with no off-site glare spill-over. Excessive brightness and brilliant colors shall not be used unless clearly demonstrated to be integral to building architecture*
- A combination of building-mounted lighting and pole-mounted lighting is proposed, with high lighting levels proposed within the shared drive aisle and parking areas. This will promote safety in the surface parking area on the south side of the building, to be used by those apartment residents and commercial tenant occupants/visitors who do not have a dedicated space within the parking garage. The cut sheets below are the proposed light fixtures. The Tukwila Police Department reviewed the proposed lighting levels and agreed that lighting levels are high enough to promote safety according to Crime Prevention Through Environmental Design (CPTED) principles.

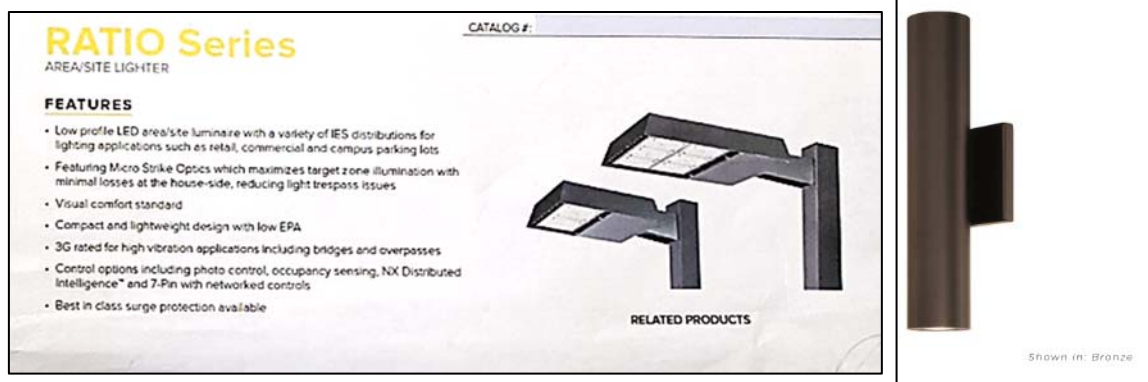


Figure 17: Lighting Fixtures

PUBLIC COMMENTS

Two public comment letters were received by email from neighbors who live on properties above the site which include the adjacent hillside. Letters were submitted by Michelle Ricks (14428 59th Ave S) and Benjamin Boyles (14234 59th Ave S). Original copies of the letters and Mr. Galliano's responses to the letters are included as Attachment B and are summarized below. Both neighbors had variety of concerns about the project, however the main areas of concern were the project's potential impact to hillside stability (1), the potential removal of trees from the hillside (2), and a concern about how garbage would be handled to prevent attracting pests to the site and surrounding area (3).

1. Hillside Stability

Staff coordinated with the applicant, who confirmed that the proposed grading and site work will not disturb the hill and will follow the recommendations of the geotechnical report prepared for the project. A follow-up letter was provided from the geotechnical engineer, who stated the following: "The proposed excavation for the building foundation footings will **not** adversely affect slope stability." Additionally, "On site geotechnical inspections are required to verify that actual construction complies with the City of Tukwila approved plans, the requirements of the Civil Engineer and this Geotechnical Engineer."

2. Potential for Tree Removal on the Hillside

Sheet L1.2 of the landscaping plan set submitted for the project shows tree protection fencing along the inside of Mr. Galliano's property in vicinity of the project site. Two trees which appear to straddle the property line will remain and will be protected during construction. Mr. Galliano confirmed in responses to comments that "no hillside or tree cutting exists for this project."

Per the Environmentally Critical Areas Chapter of the Tukwila Municipal Code (TMC 18.45), tree removals on the hillside require permits due to their location within a steep slope critical area. City code related to tree removals within critical areas has recently been updated to include penalties and remedial measures for removal or damage of trees(s) without applying for and obtaining required City approval. Penalties include a fine of \$1,000 per tree, or up to the marketable value of each tree removed or damaged as determined by a Qualified Tree Professional, whichever is greater. Additionally, the Director shall require any person conducting work in violation of the critical areas chapter to mitigate the impacts of unauthorized work by carrying out remedial measures.

3. Garbage

The trash and recycling location for the new building is located inside of the ground floor parking garage. Waste Management reviewed the proposed location and confirmed that the design of the trash and recycling area is feasible for trash collection. Mr. Galliano's response stated he is not aware of a rodent issue. However, it is typical for there to be rodents in urban and residential areas of Tukwila. It is important for businesses and residents to keep trash secure to try to keep from attracting rodents and other pests.

CONCLUSIONS

Site Planning:

- A network of sidewalks accommodates pedestrian access separated from vehicular traffic by curbs and landscaping. Landscaping helps to frame pedestrian entries to the site, accentuate building entryways, and create privacy.
- Parking and service areas are largely screened from view due to their location either within the parking garage on the first floor of the proposed building, or at the rear side of the property where their prominence is minimized.
- An area of dense vegetation including many significant trees is located on the hillside owned by the residential property owners to the west. This area is not proposed to be disturbed with the current project, and will continue to support slope stability, screening, and separation of development along Interurban Ave S from adjacent residential properties on the hillside adjacent to the project's west/rear side.

Building Design:

- The building design and scale blends the predominantly commercial development typical of the RCM and C/LI zones along Interurban Ave S with the residential development located on the hill above the site, as well as the apartments along Interurban within the MDR zone to the north.
- The building includes both vertical and horizontal modulation. Vertical modulation is provided in a symmetrical design along the front of the building adjacent to Interurban Ave S. On the rear side of the building where it is adjacent to residential properties and the LDR zone, a horizontally tiered setback is proposed as required by zoning setbacks to reduce the bulk of the building.
- The flat roof design is consistent with nearby commercial development. The inclusion of roof overhangs and corbels, along with a rich, warm color scheme that makes use of stone veneer, lap siding, and trim is consistent with nearby multifamily and single-family residential development.

Landscaping and Site Treatment:

- Existing street trees and mature landscaping is proposed to be retained to the extent possible. Six trees are proposed to be removed and will be replaced with landscaping.
- Perimeter landscaping is proposed as required along the front, second front, and rear of the property. A narrow landscape strip adjacent to the shared drive aisle is proposed. Existing landscaping on other areas of the site (associated with the restaurant and the parking area on the south side of the restaurant) is proposed to remain as-is.
- Landscaping plans show tree protection fencing adjacent to the hillside to protect trees on the adjacent properties.

Miscellaneous Structures

- Miscellaneous structures proposed are the bike racks and the concrete seating wall planter, which are shown to be in front of the building where they will be visible from the commercial tenant space.
- The location and space provided for the bike rack is not shown consistently among different site plans, and only one bike rack is required. Staff recommends a condition to require that the details of the bike parking design be worked out as part of the building permit, to provide the required number of bike parking spaces while at the same time preserving as much green space as possible in front of the building.
- The trash and recycling bins are proposed to be stored inside the parking garage where they will be screened from view.
- Mechanical plans submitted for the project show that there will be equipment visible on the building exterior, including vents, exhausts, and louvres. No rooftop mechanical equipment is shown at this time, and utility boxes and meters are also not shown. As a condition of approval, staff recommends that all mechanical equipment located on the building exterior be painted to match the color scheme of the building. If any rooftop mechanical equipment is necessary, it shall be setback from the edge of the roof and screened by the height of the parapet or otherwise screened from view. Ground equipment, including utility boxes and meters, shall be screened by project landscaping. Additional landscaping from what is shown on the plans may need to be added during project construction to screen this equipment since it has not yet been shown on the plans and is often added to a site after the building permit has already been issued.
- A combination of building-mounted lighting and pole-mounted lighting is proposed, with high lighting levels proposed within the shared drive aisle and parking areas. The Tukwila Police Department reviewed the proposed lighting levels and agreed that lighting levels are high enough to promote safety according to Crime Prevention Through Environmental Design (CPTED) principles.

RECOMMENDATIONS

Staff recommends approval of the construction of the Galliano Mixed Use Building with the following condition(s):

1. Details of the bike parking design, including the rack and the size of the concrete pad, shall be worked out as part of the building permit to provide the required number of bike parking spaces while at the same time preserving as much green space as possible in front of the building.
2. Mechanical equipment located on the building exterior shall be painted to match the color scheme of the building.
3. If any rooftop mechanical equipment is necessary, it shall be setback from the edge of the roof and screened by the height of the parapet (or otherwise screened from view).
4. Ground equipment, including utility boxes and meters, shall be screened by project landscaping. Additional landscaping from what is shown on the plans may need to be added during project construction to screen this equipment since it has not yet been shown on the plans and is often added to a site after the building permit has already been issued.